

## ASBA ZONE 2/3

### AGENDA

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#### St. Anthony Centre

10425 84 Ave NW, Edmonton, AB, T6E 2H3

**April 26, 2019 – 9:30 a.m. – 3:30 p.m.**

*(Lunch will be provided - Please advise if you have any dietary concerns)*

9:00 AM Coffee | Registration | Networking

9:30 AM Zone Business Meeting

**Please be prepared for this to be a full day meeting**

1. Call to Order/Recognition of Treaty 6 Land
2. Consideration and Approval of the Agenda
3. Approval of Minutes of General Meeting held on March 15, 2019 - Attached
4. Zone Business:
  - 4.1. Standing Item – ASBA Issues/Zone Request for Action
  - 4.2. Chair’s Report – Colleen Holowaychuk – Attached
    - 4.2.1. Request for an ASBA representative to participate on the King’s University Teacher Education Advisory Committee (TEAC).
  - 4.3. Changes to 2019/2020 Proposed Meeting Schedule - Attached
  - 4.4. Initial Discussion of Zone Proposed Position Statements - Attached
5. ASBA Reports
  - 5.1. President Lorrie Jess or Vice President Trina Boymook
  - 5.2. Chief Executive Officer – Dr. Vivian Abboud
  - 5.3. Zone Directors – Cheryl Dumont, Arlene Hrynyk
6. Technology, Students, and the Classroom – 10:30 a.m.  
Dr. Philip McRae
7. ASBA Budget Presentation – 1:00 p.m.  
President Lorrie Jess and CEO Dr. Abboud
8. Zone Committee Reports:
  - 8.1. Edwin Parr Report  
*Joe Becigneul, Jean Boisvert, Cathie Langmead*
  - 8.2. U of A Field Experiences Policy Advisory Committee Report  
*Glenys Edwards*
  - 8.3. Edmonton Regional Learning Consortium (ERLC) Report  
*Diane Bauer*
  - 8.4. Zone Language Report  
*Gemma Christie, Sonya Vincent*

**9. Board Sharing – Time permitting**

*This is an opportunity to share achievements, celebrations and challenges with other boards. Please provide our Secretary Kim Ayers a brief outline of your message for our minutes.*

**10. Adjourn**

**Next Meeting**

**General Meeting: May 24, 2018, 12:30 pm**

Chateau Louis Hotel and Conference Centre  
11727 Kingsway NW, Edmonton

**Edwin Parr Awards Evening May 24, 2018, 6 pm**

Chateau Louis Hotel and Conference Centre  
11727 Kingsway NW, Edmonton

**ASBA ZONE 2/3**

**LAST MEETING MINUTES FOR APPROVAL**

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**ASBA ZONE 2/3**  
**DRAFT MINUTES**

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**St. Anthony Centre**

*10425 84 Ave NW, Edmonton, AB, T6E 2H3*

**March 15, 2019 – 9:30 a.m. – 3:00 p.m.**

9:00 AM Coffee | Registration | Networking

9:30 AM Zone Business Meeting

**1. Call to Order/Recognition of Treaty 6 Land**

**2. Consideration and Approval of the Agenda 9:30 a.m.**

- Added “ASCA AGM” as 4.6
- 8.2 ERLC and 8.3 Zone Language Committee will not have reports.

**MOTION: Trustee McBride (Evergreen) moved to approve the Agenda.**

**Carried.**

**3. Approval of Minutes of General Meeting held on February 22, 2019 - Attached**

**MOTION: Trustee J. Lefebvre (Pembina Hills) moved to approve the minutes of the general meeting held on February 22, 2019.**

**Carried.**

**4. Zone Business:**

**4.1. Standing Item – ASBA Issues/Zone Request for Action**

**4.2. Chair’s Report – Colleen Holowaychuk – Attached**

**4.3. 2019/2020 Proposed Meeting Schedule - Attached**

**4.4. Zone 2/3 Proposed Position Statement Process - Attached**

**4.5. Initial Discussion of Zone Proposed Position Statements**

**4.6. ASCA AGM**

- Zone discussed concerns regarding the resolution process.

**5. Alberta Education Report – Nathan Freed or Garrett Doll, Alberta Education**

See *In the Loop* for current information. Under **Ministry** click “**In The Loop**” <https://education.alberta.ca/>

- Minister announced budget submission deadline extended to June 30, 2019.
- High school redesign – For more information school authority leaders should contact Debbie Rowley
- The Deputy Minister sent an email to Superintendents regarding seclusion rooms. Each school authority is asked to submit which schools in their authority have one or more seclusion rooms. Asked to identify those rooms and then put a plan in place to deal with them. Due by March 29, 2019. Working on a process for submitted signed declaration forms that seclusion rooms have been decommissioned.
- Effective March 1 any driver wanting Class 1 must have MELT training.  
Alberta Education has been meeting regularly with Alberta transportation. Certainly heard the concerns across Alberta regarding concerns with this.
- S endorsement: Alberta Transportation will give July 31 extension for those who need. Recognize the increased costs associated with this. Alberta Transportation will license school authorities and private contractors to do the training in an effort to help.
- March 13, email sent to Board Chairs regarding extending school fees and transportation rates. Were set to expire but now extended.

- Election season beginning. Government staff cannot be seen as promoting certain work. Staff will be quite quiet and not able to comment as much.
- 3 year education plan. A sincere appreciation regarding the results report. Excellent opportunities for the department to understand the work. Appreciate all the efforts and dialogue with senior leadership to understand what is happening at the local level.
- Our website is going through a change. Double check links to ensure they are working.

**6. Unpacking the New Curriculum – 10:30 a.m.**

- Michele Jones, M.Ed. Curriculum Implementation Consultant, Wellness Education Consultant
  - New Curriculum can be found here: <https://new.learnalberta.ca>
  - Resources: [www.erc.ca](http://www.erc.ca)  
PD resources → Featured websites → curriculum implementation for Alberta educators.
  - Summary Overview document K-4:  
Summary overview and What is Changing links <https://www.alberta.ca/curriculum-development.aspx>
  - ERLC Presentation Slides: <http://bit.ly/2FgaUOa>

**7. Generative Discussion – Personal Devices in School – Approximately 1:00 p.m. for 1 hour**

**8. Zone Committee Reports:**

- 8.1. Advocacy Committee Report** - Paula Galenzoski, Margaret Border, Irene Sawyer, Anne Karczmarczyk  
Final advocacy documents were shared. These documents are to be used to inform conversation but are not to be handed out to the public. However, the “What School Boards Do” may be shared.

**9. Board Sharing – Time permitting**

*This is an opportunity to share achievements, celebrations and challenges with other boards.*

**10. Adjourn**

**MOTION: Trustee Sawyer (*Living Waters*) motioned to adjourn the meeting 1:45 p.m.**

**Carried.**

**Next Meeting**  
April 26, 2019  
St. Anthony Centre

**ASBA ZONE 2/3**

**REPORTS**

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## Zone 2/3 Chair's Report – April 2019



I hope this Chair's report finds everyone well and excited to celebrate Easter!

April has proven to be a very busy month and you can expect a very full day at our Zone meeting next week.

I've had to make an adjustment to our PD this month as it has been requested by ASBA for approximately 90 minutes on the agenda for budget discussions. President Lorrie Jess and CEO Dr. Abboud will attend the Zone meeting in person for this. We had Charlene Bearhead scheduled for April and she has agreed to come to our May meeting instead. Although we have said that we will not have PD in May, it is important that we take time to fit this piece in before the end of the year.

### **Personal Devices and Role of Zones Generative Discussion**

Thank you everyone for your participation in the generative discussion at our last meeting on personal devices in the classroom. I have compiled the text and will have it available for you. Some boards are beginning to think about policies regarding personal devices and hopefully you will find that both the discussions and compiled text will be helpful as you plan. Please feel free to use what you need for your work within your board. I would ask that the piece around what individual boards are or are not doing regarding a policy is not shared outside of your boardwork.

The compiled text from the Role of Zones has been shared with ASBA for their work. Both documents of the information gathered from the discussions will be shared with you in a separate email from this agenda.

### **Proposed 2019/2020 Meeting Schedule**

We have attached the proposed meeting schedule for 2019/2020. We will be approving the meeting schedule in May. Please take the time to look at the dates and provide input at the meeting.

The St Anthony's Meeting Centre is not available for the September 27, 2019 so we changed it to Sept 13, 2019. It is also not available on January 24, 2020 so we've made the change to January 17, 2020.

Please see the proposed meeting dates which are attached to the agenda. We will vote on the proposed schedule at our May Zone meeting.

### **Call for Proposed Policy Position Statements (formerly known as proposed policies)**

We will have discussions on the proposed position statements at the April zone meeting and then we will vote on which ones to submit to the ASBA Policy Advisory Development Committee (PDAC) at the May zone meeting.

If you were unable to meet the April 12, 2019 deadline to have it included in the agenda, please bring 50 printed copies to the April 26, 2019 meeting.

### **Request for an ASBA representative to participate on the King's University Teacher Education Advisory Committee (TEAC)**

We have been asked to determine a course of action on whether or not we would like to have a Zone 2/3 representative to this committee.

PDAC is currently reviewing Governance Policy 9: Board Representatives to External Committees. Until that review is complete, ASBA is following the current practice, which is that post-secondary education faculty committees are zone directed and administered appointments. As such, the decision to appoint a representative to the King's University TEAC will be at our zone's discretion.

The meeting is scheduled to take place on May 17, 2019 in Edmonton.

Please take some time to consider this and bring your thoughts forward to the April meeting. If the decision is to have a representative we can either hold a bi-election or appoint someone to attend the upcoming meeting and wait until a review is done or until our regular elections at the annual general meeting in October 2019.

### **June Zone 2/3 Meeting**

It is the time of year that we start thinking about whether or not we want to hold a June Zone meeting. The scheduled date is June 21, 2019. Please start thinking about it as we will make a decision at our May meeting.

### **Upcoming Professional Development**

#### **April**

Technology, Students, and the Classroom  
Dr. Philip McRae

#### **May**

First Nations, Metis and Inuit Relations Coordinator, Charlene Bearhead (*postponed from April*)

(Charlene will spend some time with us to answer any of our questions and share thoughts about the Indigenous Relations, engaging Indigenous families and community, Indigenous representation in our schools, supporting Indigenous students, how to approach policy change in keeping with Call to Action 62 from the TRC, Honouring Spirit Awards or any other topics that we would like to discuss).

**Upcoming Events/Deadlines**

<b>Date</b>	<b>Event/Deadline</b>
April 26, 2019	Zone 2/3 Meeting
April 26-28, 2019	Alberta School Council Association Conference & AGM, Edmonton
April 26, 2019	Zone 2/3 Meeting St Anthony's Center, Edmonton
May 24, 2019	Zone 2/3 Meeting Chateau Louis Hotel Conference Centre 12:30pm
May 24, 2019	Edwin Parr Celebration Chateau Louis Hotel Conference Centre
June 21, 2019	Possible Zone 2/3 Meeting St Anthony's Center, Edmonton
July 3-7, 2019	2019 National Trustee Gathering on Indigenous Education and CSBA Congress Toronto, Ontario Westin Harbour Castle Hotel

Respectfully Submitted by:  
Colleen Holowaychuk, Chair, ASBA Zone 2/3  
780-722-5370 / colleen.holowaychuk@eips.ca

## ASBA ZONE 2/3

### 2019-2020 PROPOSED MEETING SCHEDULE

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#### GENERAL MEETING

~~September 27, 2019 – St. Anthony Centre~~

September 13, 2019 – St. Anthony Centre

November 29 or December 6, 2018 – St. Anthony Centre

~~January 24, 2020 – St. Anthony Centre~~

January 17, 2020 – St. Anthony Centre

February 21, 2020 – St. Anthony Centre

March 20, 2020 – St. Anthony Centre

April 24, 2020 – St. Anthony Centre

May 22, 2020\*\* - Chateau Louis, 12:30 p.m.

June 19, 2020 – St. Anthony Centre

*\*MLA Advocacy Presentation – to be determined Date & Format*

*\*\*Edwin Parr Awards Banquet – May 22, 2020 (Evening)*

#### GENERAL INFORMATION

General Meetings will commence at 9:30 a.m.

Annual Meetings commence at 9:30 a.m.

#### MEETING LOCATIONS

St. Anthony Centre

10425-84 Avenue, Edmonton

Chateau Louis Hotel & Conference Centre

11727 Kingsway NW, Edmonton, AB

**ASBA Zone 2/3 Proposed Policies**

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**Alberta School Boards Association (ASBA) Proposed Position Statement**  
**Submitted by: Parkland School Division**  
**April 2019**

Position Statement: That Alberta School Boards Association urge Alberta Education to develop a *Standards for an Inclusive Education in Alberta* and rescind the *Standards for Special Education, Amended June 2004*.

**Background Research**

The *Standards for Special Education, Amended June 2004* is a ministerial order that applies to public and separate school boards and designated special education private schools. It outlines program delivery requirements and expectations that support Alberta's objective of providing consistent direction while supporting flexibility and discretion at the local level.

In order to meet the needs of students, programming

- is based on an individualized program plan (IPP) or instructional support plan (ISP); all students with special needs, including students who are gifted and talented, require IPPs/ISPs
- may be provided in a variety of settings
- is based on information gathered, including the results of ongoing assessment, observation and evaluation
- provides access to an educational program that is relevant, meaningful and age-appropriate, using the programs of study as a starting point of instruction.

*Guide to Education, ECS to Grade 12, 2018-2019 (p38).*

School boards are still expected to comply with this fifteen-year old ministerial order that outlines the requirements regarding the delivery of education programming and services to students with special education needs in Grade 1 to Grade 12.

As a result of the Alberta government's *Setting the Direction: Government of Alberta Response* in 2010, Alberta Education and school authorities have moved to the creation of a new paradigm of inclusive learning environments where all students, including those with special education needs, are valued, recognized and supported.

Alberta Education developed an Inclusive Education Policy: *Alberta's education system is built on a values-based approach to accepting responsibility for all children and students. Inclusion is a way of thinking and acting that demonstrates universal acceptance of, and belonging for, all children and students. To support children and students in attaining the goals as stated in the Ministerial Order on Student Learning, school authorities must ensure that all children and students (Kindergarten to Grade 12), regardless of race, religious belief, colour, gender, gender identity, gender expression, physical disability, mental disability, family status or sexual orientation, or any other factor(s), have access to meaningful and relevant learning experiences that include appropriate instructional supports (Guide to Education, ECS to Grade 12, 2018-2019, p31).*

Alberta Education also provided school authorities with further detail, direction and resources regarding inclusion: *Inclusion is not just about learners with disabilities or exceptionalities. It is an attitude and approach that embraces diversity and learner differences and promotes genuine equality of opportunities for all learners in Alberta. Every learner has unique needs.*

*Some learners have profound and ongoing needs; others have short-term and/or situation-based needs. This calls for flexible and responsive learning environments. An inclusive education system that responds to the needs of all learners addresses factors that contribute to the achievement gap and other negative impacts that result from poverty, social exclusion, marginalization, underachievement and mental health issues. Alberta Education has produced a number of resources that include information and strategies for addressing the diverse learning needs of children and students and that support the implementation of the Inclusive Education Policy (Guide to Education, ECS to Grade 12, 2018-2019, p37).*

In addition, in 2012, Alberta Education replaced the Special Education Funding Model with an Inclusive Education Funding Model providing funding to support student needs, including, but not limited to, those students with special education needs.

At the school authority level, changes were also occurring. Many divisions reconfigured their division offices to align with the direction of *Setting the Direction: Inclusive Education* departments replaced Special Education departments and a continuum of supports and services was implemented to support all learners. Significant strides were made in Alberta school authorities to ensure that all students are meaningfully included in school life and the practice of integrating students into typical learning environments is prevalent.

It is clear that there is a focus in the Alberta government and schools on inclusive education - valuing all students regardless of their diverse or special learning need. However, the *Standards for Special Education, Amended June 2004*, continues to put the focus on specific requirements for school boards related to students identified with special education needs.

As a result of this focus, school boards face an awkward dilemma: how to enhance and support all learners when boards are held to a standard specifically for students with special education needs? In other words, we feel that the *Standards for Special Education, Amended June 2004* is a barrier to the promotion of inclusive education environments, supports and programming for all students. School boards are still required to comply with the *Standards for Special Education, Amended June 2004*, which results in the reinforcement of difference through a special education lens versus diversity through an inclusive education lens.

The *Standards for Special Education, Amended June 2004* requires boards to identify those students with special education needs and provide appropriate and relevant programming, where boards really need to identify all student needs and provide appropriate and relevant programming for all. However, school boards are required to provide appropriate and relevant educational opportunities to all their resident students. To support school boards in their endeavor, a *Standards for Inclusive Education in Alberta* would outline requirements for school boards regarding the provisions of supports and services for all learners consistent with Alberta Education's Inclusive Education Policy.

A draft *Standards for an Inclusive Education in Alberta* was developed by a sub-committee of the College of Alberta School Superintendents and presented to Alberta Education's Building an Inclusive Education Advisory Committee in the spring of 2017. The draft follows the format of the *Standards for Special Education, Amended June 2004*; however, it outlines the requirements for school boards regarding the provision of a continuum of specialized supports and services to students that is consistent with the principles of inclusive education. The College of Alberta School Superintendents has not received notification of the status of the draft.

We believe there is strong support for an inclusive education system in which all learning environments effectively support diverse learners, and the Standards for an Inclusive Education in Alberta would provide guidance and direction to school boards to achieve that goal.

**Position Statement:** That Alberta School Boards Association urge Alberta Education to develop a *Standards for an Inclusive Education in Alberta* and rescind the *Standards for Special Education, Amended June 2004*.

References:

[Guide to Education, ECS to Grade 12, 2018-2019](#)  
[Standards for Special Education, Amended June 2004](#)  
Draft Standards for an Inclusive Education in Alberta

## **Alberta School Boards Association (ASBA) Proposed Policy Position Statement**

**Submitted by: Pembina Hills Public Schools    Secondar: Parkland School Division**

### **Policy Position Statement**

That the Minister of Transportation establish that school buses may operate at the same speed as the posted speed limit, as prescribed by law.

### **Background Research**

School boards are to provide transportation to eligible students. Transportation to and from school shall be provided in a safe, effective and efficient manner consistent with provincial guidelines. In the 2017-2018 school year, there were 96,589 rural students transported daily by school bus in this province. Safety of students, is a priority and school bus drivers have an excellent safety record with many procedures to follow. The Student Transportation Association of Alberta (STAAA) has been advocating for the Ministry of Transportation to review the speed limits that school buses can travel since 2002. They have written letters to government ministers and collected observational reports from bus drivers. This is an opportunity for locally elected boards to advocate politician to politician for an issue that has been brought forward by our transportation staff and partners.

Canadian speed limits differ depending on the level of government under which the jurisdiction of the road falls. School bus drivers monitor road and weather conditions and adhere to signs just as any other driver. Alberta Transportation determines school bus operating requirements. Alberta is the only province that has a transportation regulation that guides the speed of school buses. The regulation can be found in the *Use of Highway and Rules of the Road Regulation – Section 70*. It states that school buses can only travel 90km/h or the posted speed limit, whichever is less. This means that school buses that travel on major highways in the province could be travelling 10-20km/h slower than the moving traffic around them. Earlier references to regulations on a speed limit for school buses date back to as early as 1979 which is forty years ago. Over forty years we have seen road improvements, an increasing number of vehicles on the road, and further safety measures on buses including strobe lights, and GPS. How many Albertans are aware that buses must travel at this slower speed due to a regulation, and are not allowed to travel the posted speed limit? This slower rate of speed can frustrate other drivers on the road.

There is no reason from a mechanical standpoint why school buses cannot travel at the posted speed limit, unless they have been manufactured with a speed governor. The provided chart contains evidence from every other province/territory and states that school buses may travel the posted speed limit. In Prince Edward Island the maximum posted highway speed is 90km/h, but in the majority of provinces and territories it is 100km/hr-110km/h. In fact, the Alberta regulation about speed only applies when the vehicle displays the word 'School Bus', if that is covered/removed they can travel the posted speed limit just like the rest of traffic. *Please see the chart on Canadian Highway Speed Limits for School Buses.*

When school buses are on roads with faster moving traffic bus drivers report that motorists perform unsafe passing maneuvers, inappropriate hand gestures, honking and expletives. Bus drivers are made to feel they are impeding traffic due to the slower speed they must travel and this creates hazards such as tailgating. *Please see the Pembina Hills Public Schools 'thoughtexchange' – bus driver observational reports.*

If the province was to establish that school buses could operate at the posted speed limit an added benefit would be a reduction in ride times. Students can spend over three hours a day on a bus traveling back and forth to school. Students and their families want to spend less time on the bus, preferring to be at home or involved in other activities. Rural divisions have school bus meeting/access points where students get on a different bus to bring them to the central junior/senior high school. These transfer buses often travel direct routes which include highway driving. Provincial highway posted speed limits range from 100-110km/h. A change in regulation to allow buses to travel the posted speed limit would address safety concerns, and have a positive impact on bus ride times.

For example, in Pembina Hills Public Schools when looking at data that compared what our bus ride times might look like if our buses could travel the posted speed limit we could save approximately seven hours off our ride times every day. If you multiply 7hours x 184 school days that is equal to 1,288hours in a school year! That seven hours per day takes into consideration all of our seventy bus routes. Our buses travel 10,331.44 kilometres per day. Being allowed to travel at the posted speed limit would affect many of our routes because most drivers need to take the highway at some point to enter any of our towns. This is significant time that students can spend in other ways and in other environments. Having buses travel at the posted speed limit would shorten the bus ride times of students leaving them more time for other activities. In addition, traveling the posted speed limit would also cut down on the time our students have to spend on the bus when traveling to field trip experiences that are not available in our rural communities.

There is currently no other ASBA policy position statement that speaks to this item. There is no cost to the government for this change, but potential savings in time and money for school divisions and families. As demonstrated by the research gathered from other provinces and territories, as well as supported by bus driver observations, we encourage you to support this ASBA policy position statement that keeps our students safe and gets them to their destination sooner.

## **Supporting Documents**

1. Traffic Safety Act – Use of Highway and Rules of the Road Regulation
2. Chart - Canadian Highway Speed Limits for School Buses
3. Pembina Hills Public Schools 'thoughtexchange' - bus driver observational reports
4. Letters of support



Province of Alberta

**TRAFFIC SAFETY ACT**

**USE OF HIGHWAY AND RULES OF  
THE ROAD REGULATION**

**Alberta Regulation 304/2002**

With amendments up to and including Alberta Regulation 241/2018

Current as of December 12, 2018

**Office Consolidation**

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**Negligence**

**68** Nothing in this Part shall be construed so as to permit

- (a) a person driving an emergency vehicle or a vehicle referred to in section 66 or 67 to drive, operate or park the motor vehicle in a manner that, considering the circumstances, is negligent, or
- (b) a person controlling or directing traffic to control or direct the traffic in a manner that, considering the circumstances, is negligent.

**Division 3  
School Buses**

**Definition**

**69** In this Division, "school bus" means a vehicle on which are displayed the words "school bus".

**Speed**

**70** No person shall operate a school bus at a speed greater than

- (a) 90 kilometres per hour, or
- (b) the speed limit prescribed by law,

whichever is the slower speed.

**Alternately flashing amber lights**

**71** Where a school bus is operating alternately flashing amber lamps, a person driving a vehicle that is approaching the school bus shall,

- (a) when approaching the school bus from the rear, if the school bus is on a highway that is divided by a median into 2 separate roadways, or
- (b) when approaching the school bus from the front or rear, if the school bus is on a highway that is not divided by a median into 2 separate roadways,

reduce the speed of the approaching vehicle so that if the approaching vehicle passes the school bus it does so in a cautious manner.

## Canadian Highway Speed Limits for School Buses

Province or Territory	Speed Limit for School Buses	Maximum Posted Provincial Speed Limit
<b>British Columbia</b>	Posted speed limit	120km/h
<b>Alberta</b> Alberta Regulation 301/2002, Section 70	90 km/h	110km/h
<b>Saskatchewan</b>	Posted speed limit	110km/h
<b>Manitoba</b> This speed limit is not set by regulation or statutory provision, but instead reflects the operating limits of most models. So in areas of Manitoba where the posted speed limit is 110 km/h, the maximum speed that a bus would travel would be 100 km/h.	Posted speed limit, up to a maximum of 100km/h	110km/h
<b>Ontario</b>	Posted speed limit	100km/h
<b>Quebec</b> No regulation for buses, manufacturers sometimes set speed governors on buses	Posted speed limit	100km/h
<b>New Brunswick</b>	Posted speed limit	110km/h
<b>Prince Edward Island</b> All highways in PEI are 90km/h, (which is the slowest in Canada)	Posted speed limit	90km/h
<b>Nova Scotia</b>	Posted speed limit	110km/h
<b>Newfoundland and Labrador</b>	Posted speed limit	100km/h
<b>Northwest Territories</b> Buses travel within towns or cities	Posted speed limit	100km/h
<b>Yukon</b>	Posted speed limit	100km/h, most are 90km/h
<b>Nunavut</b> No highway network, some communities are isolated with transportation just by air -have 25 municipalities and almost every one has bus service within their local district of education	Posted speed limit	90km/h

What are your thoughts regarding the current  
Alberta law that limits school buses to a maximum of  
90 km/h on the highway?  
15 March 2019

 49 People     38 Thoughts     410 Ratings

*"These are the thoughts from our bus drivers regarding the school bus speed limit law."*

- Shantelle Haitel, Director of Transportation, Pembina Hills Public Schools

## Here are the 20 thoughts that received the highest star ratings

**I think that drivers should be able to do the posted speed limit**

it would prevent other drivers from getting frustrated and trying to pass unsafely.

4.4 ★★★★★ (19 👤)

Ranked #1 of 20



**We seem to be creating a hazard on the highways at 90KM. Everyone is lined up to pass**

I have seen a lot of passing when it is not safe to do so caused by our slow speed

4.4 ★★★★★ (12 👤)

Ranked #2 of 20



**The limit should be raised to 100 km/hr to avoid being a hazard. Vehicles stack up behind me doing 90 and then someone gets impatient.**

To avoid collisions and avoid endangering the safety of the students, myself and others on the road

4.3 ★★★★★ (17 👤)

Ranked #3 of 20



**The speed limit should be higher, because a large vehicle traveling this slow on a highway can be a hazard.**

4.3 ★★★★★ (15 👤)

Ranked #4 of 20



**In order for drivers to be good at driving... They need to watch for hazards. Being the slow moving hazard on the highway can frustrate other**

motorists ...cause entourage of vehicles that back up behind buses. Putting a higher risk, of more travelers being at risk. Most don't want to be

4.3 ★★★★★ (13 👤)

Ranked #5 of 20



**I think changing the speed to 100km/hr is a good idea.**

I find that other drivers have a tendency to be aggressive with bus drivers as we are not driving the speed limit.

4.2 ★★★★★ (21 👤)

Ranked #6 of 20



**I think that school buses should be able to travel the same speed as regular traffic.**

I think that drivers are able to use judgement on what speeds are appropriate. Some highways that we drive on are twinned with clear flat land.

4.2 ★★★★★ (21 👤)  
Ranked #7 of 20



**I feel we should drive the posted speed limits on highways**

I feel that we are a hazard as we are 10km slower and the other vehicles are passing buses because we are slower

4.2 ★★★★★ (19 👤)  
Ranked #8 of 20



**I think it should be changed! I think it is far more dangerous going 90 kph! Drivers especially big truck drivers get annoyed when we drive too slow!**

We are always getting passed cause we drive too slow! I've tried doing 90kph and 100kph and it's safer doing the limit everyone does!

4.2 ★★★★★ (14 👤)  
Ranked #9 of 20



**Buses should travel at the same speed as highway vehicles. 100km**

To keep traffic at the same speeds. Less Backup.

4.1 ★★★★★ (18 👤)  
Ranked #10 of 20



**I feel buses should do the posted highway speed 100km**

I say this because I see frustration and aggression of drivers towards buses as they are slower and tying up the road

4.1 ★★★★★ (18 👤)  
Ranked #11 of 20



**I firmly believe that school buses should travel at the posted speed limit because they are a safety hazard/concern when they are traveling so much**

slower than the rest of the traffic- 20 kmh slower than everybody else if you are on some 4 lane highways

4.1 ★★★★★ (17 👤)  
Ranked #12 of 20



**I think that the speed limit should be 100km an hr to keep up with the traffic flow**  
Vehicles get lined up behind you and are passing to get around you and most of the time they are just about causing accidents.



**Allow to drive at posted speed limit on Highway.**  
At 90 we are a hazard. Semi's all doing 100 + passing us.



**We are trained to be professional drivers. Part of that is defensive driving. Being restricted to 90 kms creates potential bottlenecking on highways.**

Being stuck behind a vehicle as large as a bus reduces the visibility of that driver which could result in poor passing choices.

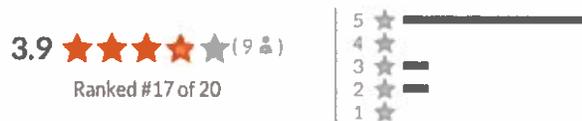


**Vehicles passing buses when unsafe to do so Creates a very high accident risk**



**I believe school buses are causing traffic problems, as we only travel at 90km other vehicles go out of their way to pass 1-2-3 buses at a time.**

Some vehicles don't even slow to pass or take their cruise control off, Or they pass with oncoming traffic coming because of our speed.



**With the impatience of drivers today they will pass when it is not safe**  
Safety



**I think the limit should be raised to 100km/hr. I believe it to be more dangerous to have vehicles passing recklessly due to impatience. More opportunity for accidents.**

3.8 ★★★★★ (8 👤)

Ranked #19 of 20



**Driving at 100 km/hr seems to keep the traffic spread out a little more and reduces back-up.**

3.8 ★★★★★ (6 👤)

Ranked #20 of 20

